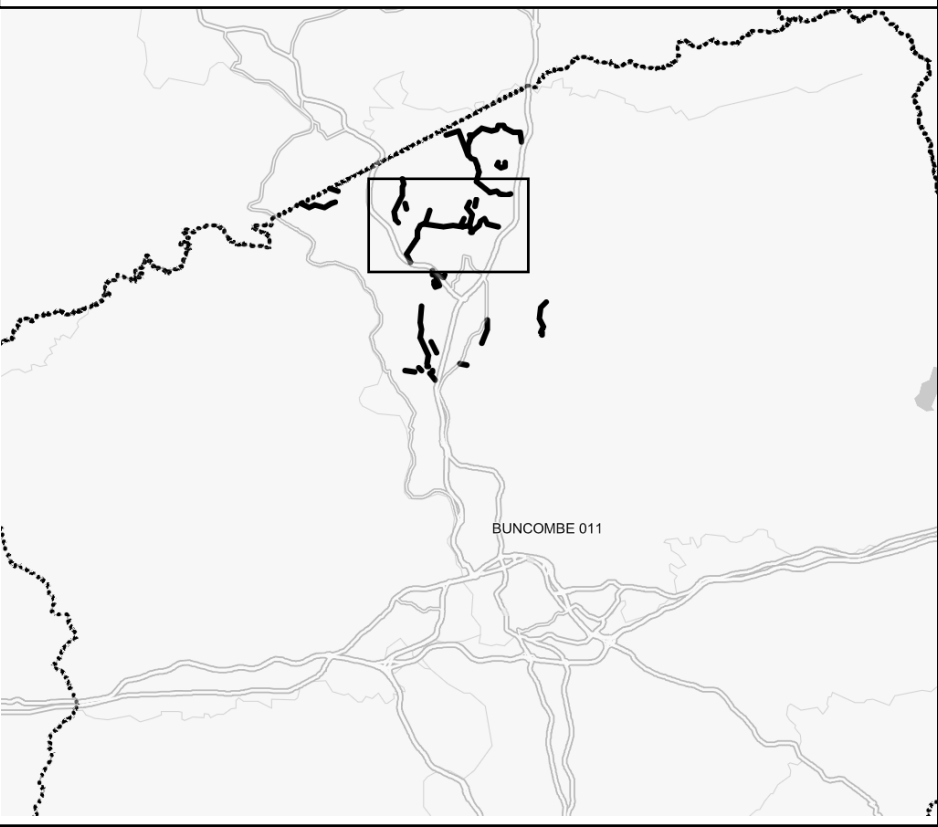


DM00474 WBS - 2025CPT.13.08.20112 SHEET 1



Map#	Route Name	FROM_DESC	TO_DESC	Length
1	SR 1733 (CLARKS CHAPEL ROAD)	SR 2207 (OLD MARS HILL HIGHWAY)	SR 1737 (CLARKS CHAPEL ROAD EXTENSION)	1.11
2	SR 1737 (CLARKS CHAPEL EXTENSION)	SR 1764 (FLAT CREEK CHURCH ROAD)	SR 1733 (CLARKS CHAPEL ROAD)	0.98
3	SR 1738 (OLD GIBBS ROAD)	SR 1737 (CLARKS CHAPEL EXTENSION)	SR 1740 (NEW STOCK ROAD)	1.32
4	SR 1740 (NEW STOCK ROAD)	US 25/70 PAVEMENT CHANGE	SR 1764 (FLAT CREEK CHURCH ROAD) PAVEMENT CHANGE	1.74
7	SR 1755 (INDIAN CAMP BRANCH ROAD)	SR 1741 (SHEPPARD BRANCH ROAD)	SR 1756 (JUPITER ROAD)	1.38
22	SR 1857 (HIGGINS ROAD)	SR 1738 (OLD GIBBS ROAD)	END OF MAINTENANCE	0.32
23	SR 1872 (SAWMILL ROAD)	SR 1741 (SHEPPARD BRANCH ROAD)	END OF MAINTENANCE	0.17
25	SR 1879 (HAMPTON DRIVE/HAMPTON LANE)	SR 1764 (FLAT CREEK CHURCH ROAD)	END OF MAINTENANCE	0.29

00.280.551.1 mi

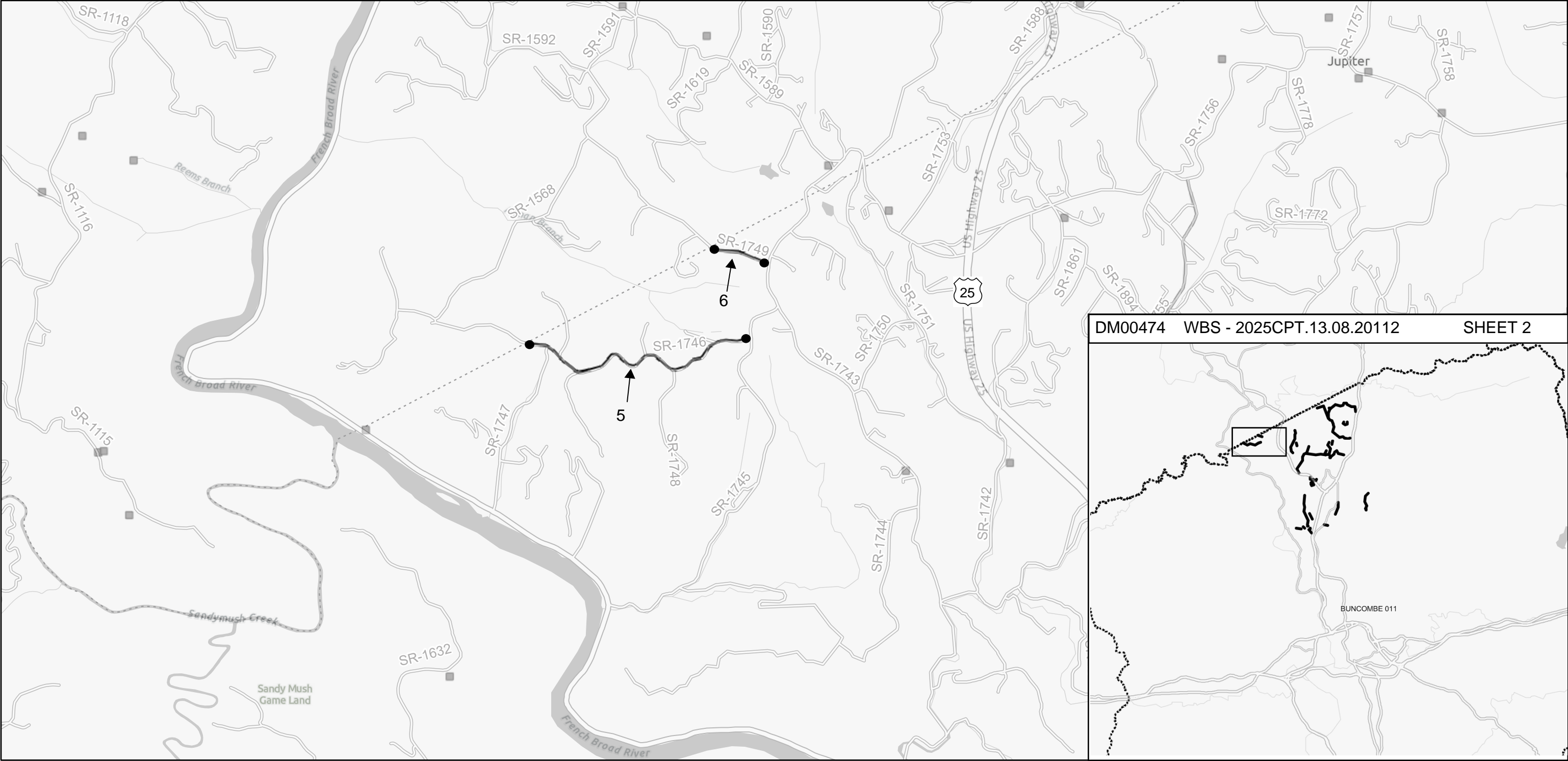
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1:30,000

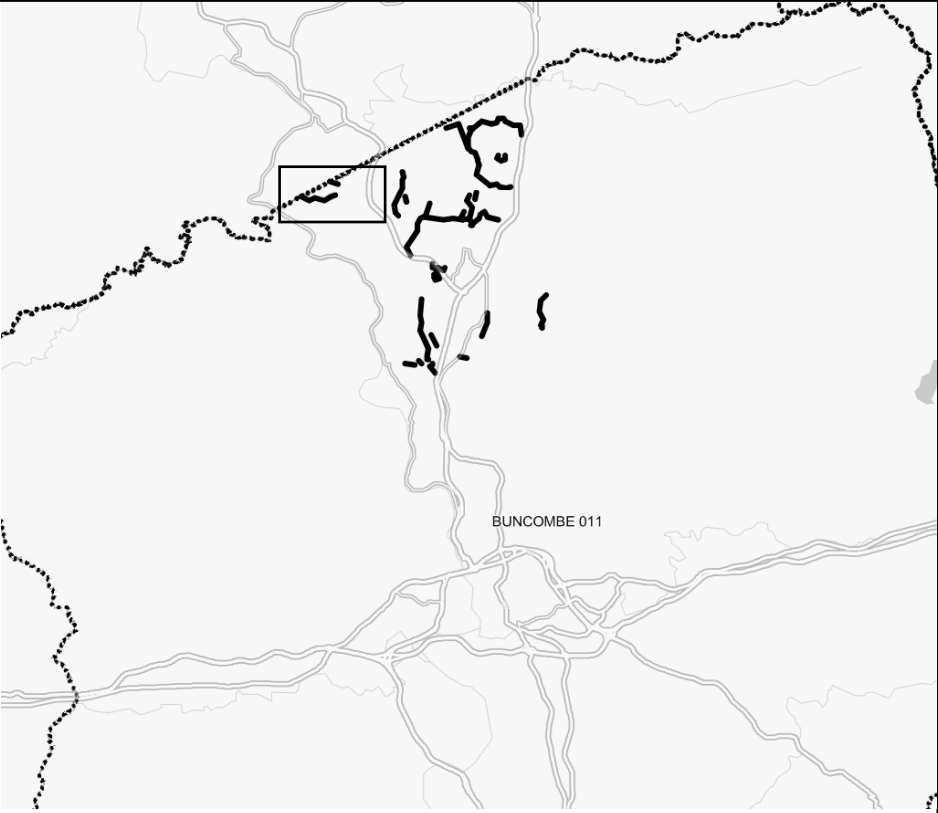
Buncombe County, NC, State of North Carolina DOT, Esri, TomTom, Garmin, SafeGraph, GeoTechnologies, Inc, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS

Historic District

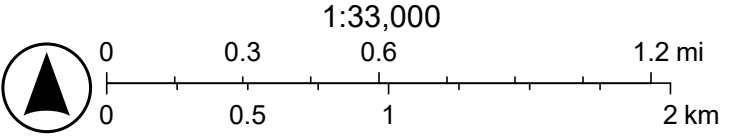
Historic Property



DM00474 WBS - 2025CPT.13.08.20112 SHEET 2



Map#	Route Name	FROM_DESC	TO_DESC	Length
5	SR 1746 (HUGHEY ROAD)	MADISON COUNTY LINE	SR 1745 (PANTHER BRANCH ROAD)	1.16
6	SR 1749 (GRANDVIEW ROAD)	SR 1743 (FLINT HILL ROAD)	MADISON COUNTY LINE	0.26



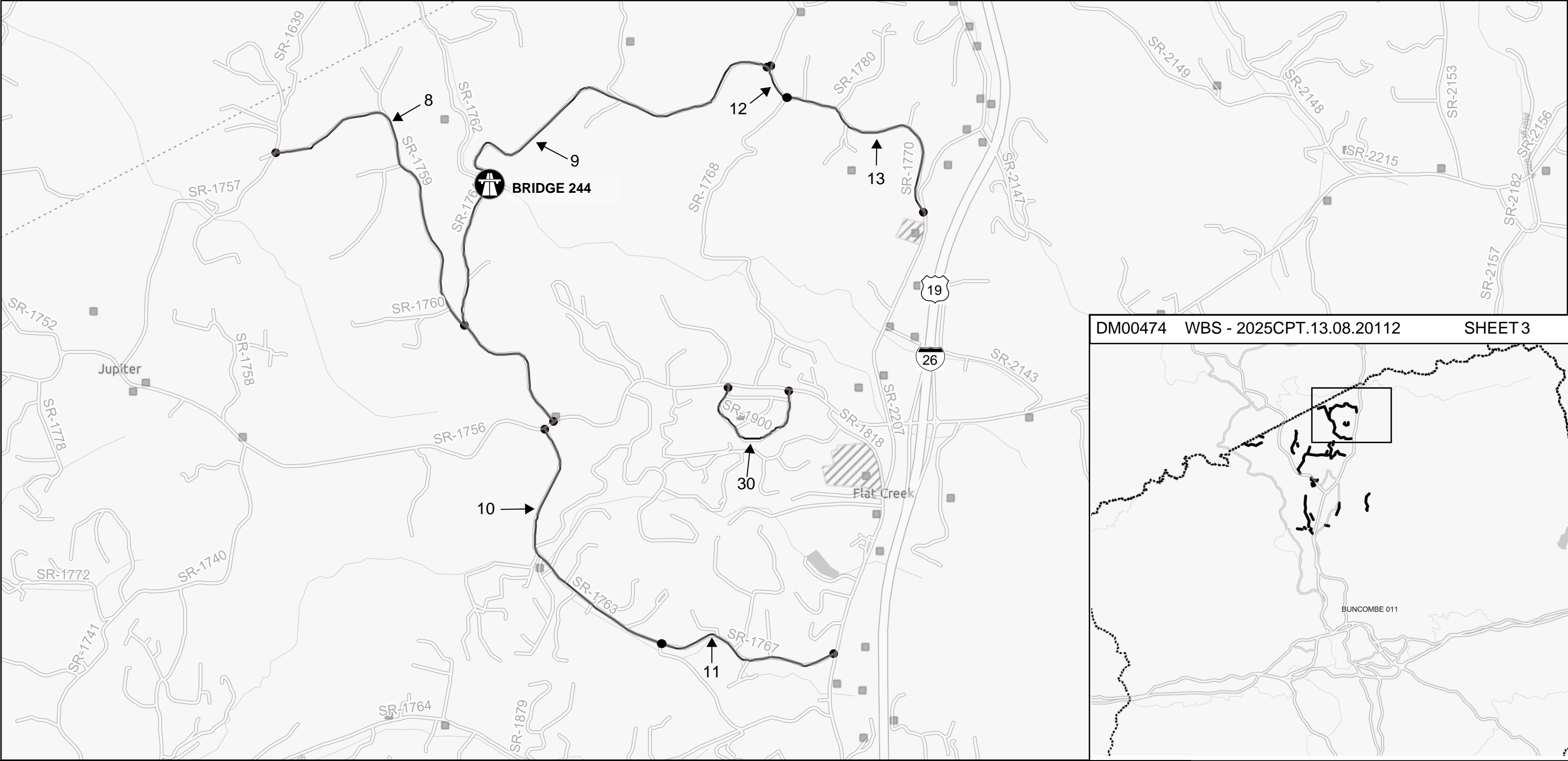
Buncombe County, NC, State of North Carolina DOT, Esri, TomTom, Garmin, SafeGraph, GeoTechnologies, Inc, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS



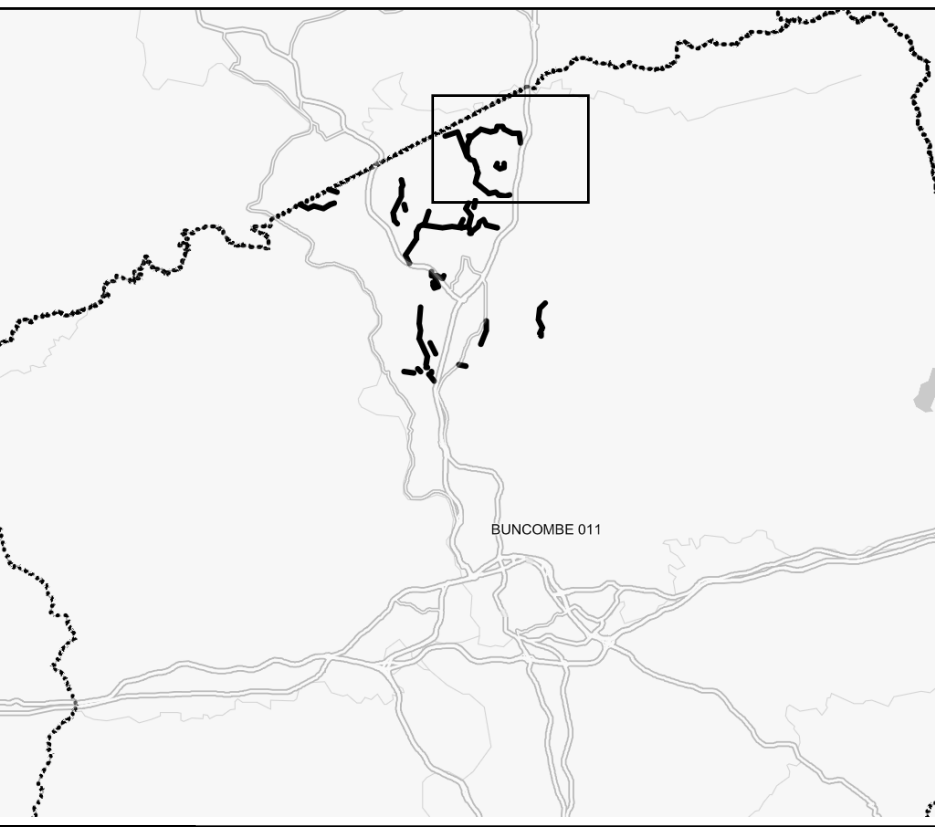
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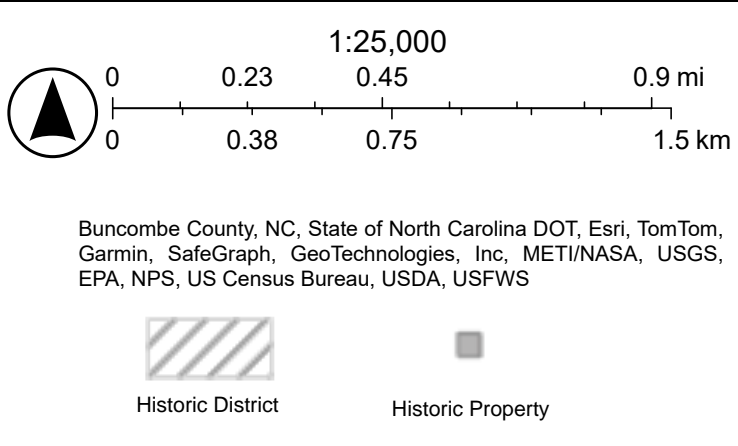
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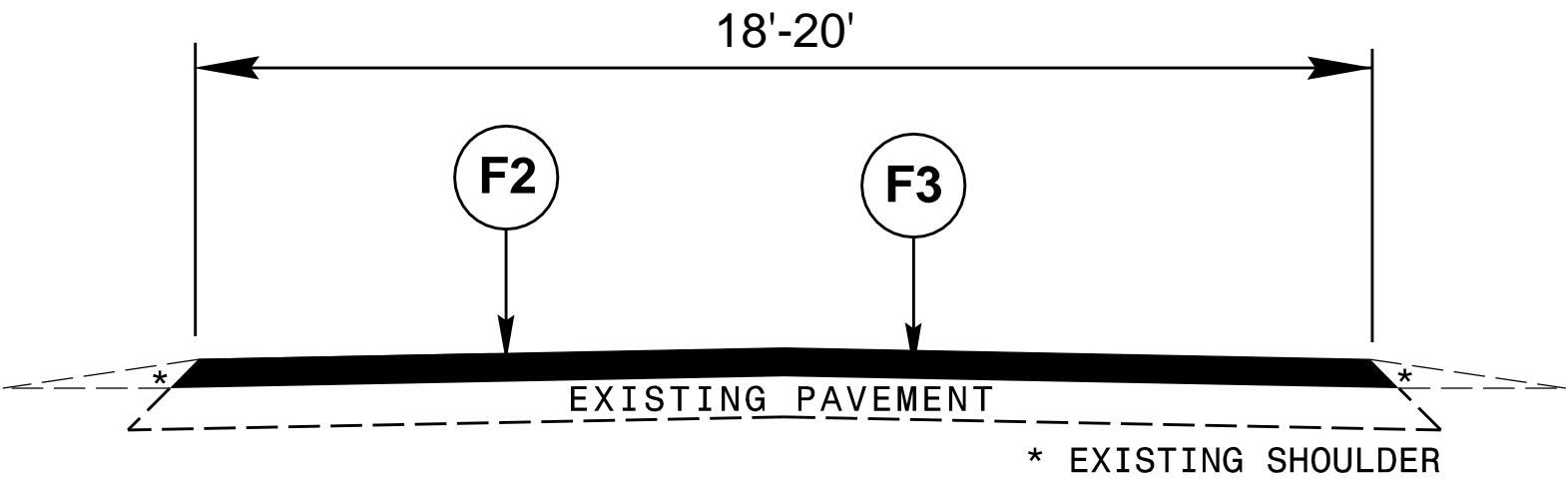


DM00474 WBS - 2025CPT.13.08.20112 SHEET 3

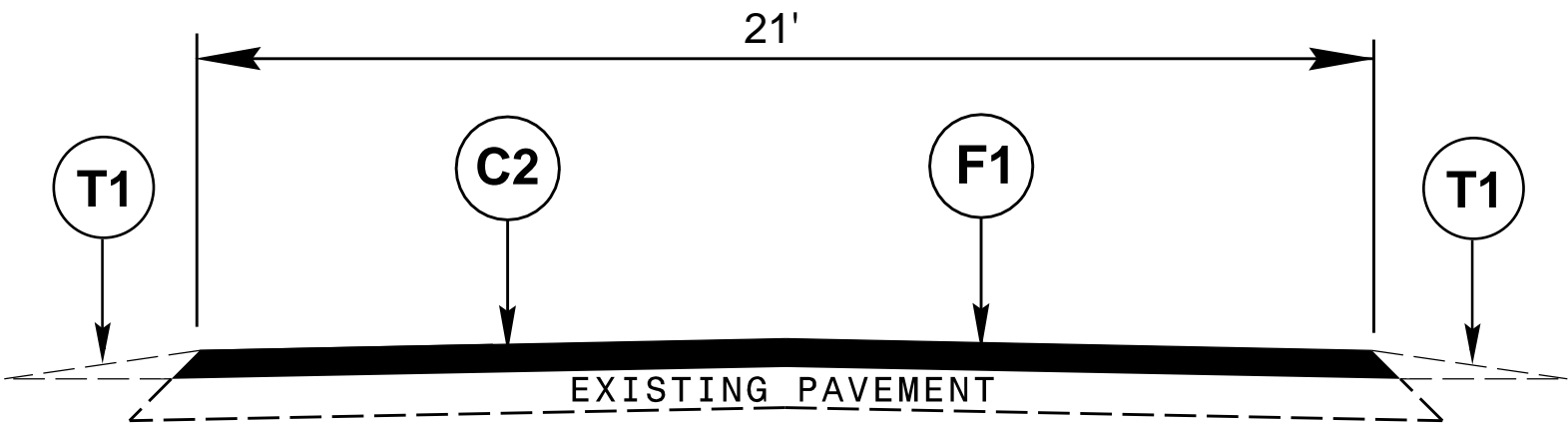


Map#	Route Name	FROM_DESC	TO_DESC	Length
8	SR 1759 (PICKENS ROAD)	SR 1757 (PALMER FORD ROAD)	SR 1761 (LOCUST GROVE ROAD)	1.16
9	SR 1761 (LOCUST GROVE ROAD)	SR 1756 (JUPITER ROAD)	SR 1768 (OLD BURNSVILLE ROAD)	2.27
10	SR 1763 (MCLEAN ROAD)	SR 1756 (JUPITER ROAD)	SR 1763 (BELL SPRINKLE ROAD)	0.95
11	SR 1767 (MCLEAN ROAD)	SR 1763 (BELL SPRINKLE ROAD)	SR 2207 (OLD MARS HILL HIGHWAY)	0.62
12	SR 1768 (LOCUST GROVE ROAD)	SR 1768 (ALLMAN HILL ROAD)	SR 1768 (OLD BURNSVILLE ROAD)	0.12
13	SR 1770 (LOCUST GROVE ROAD)	SR 2207 (OLD MARS HILL HIGHWAY)	SR 1768 (ALLMAN HILL ROAD)	0.75
30	SR 1900 (JUPITER HILLS DRIVE)	SR 1756 (JUPITER ROAD)	SR 1756 (JUPITER ROAD)	0.47

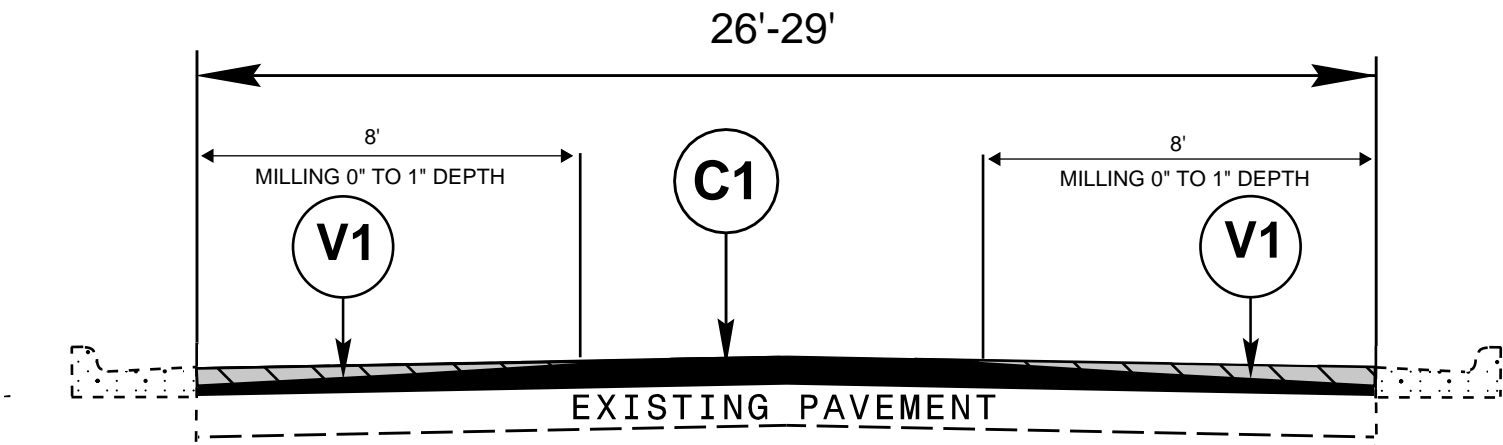




TYPICAL SECTION #1

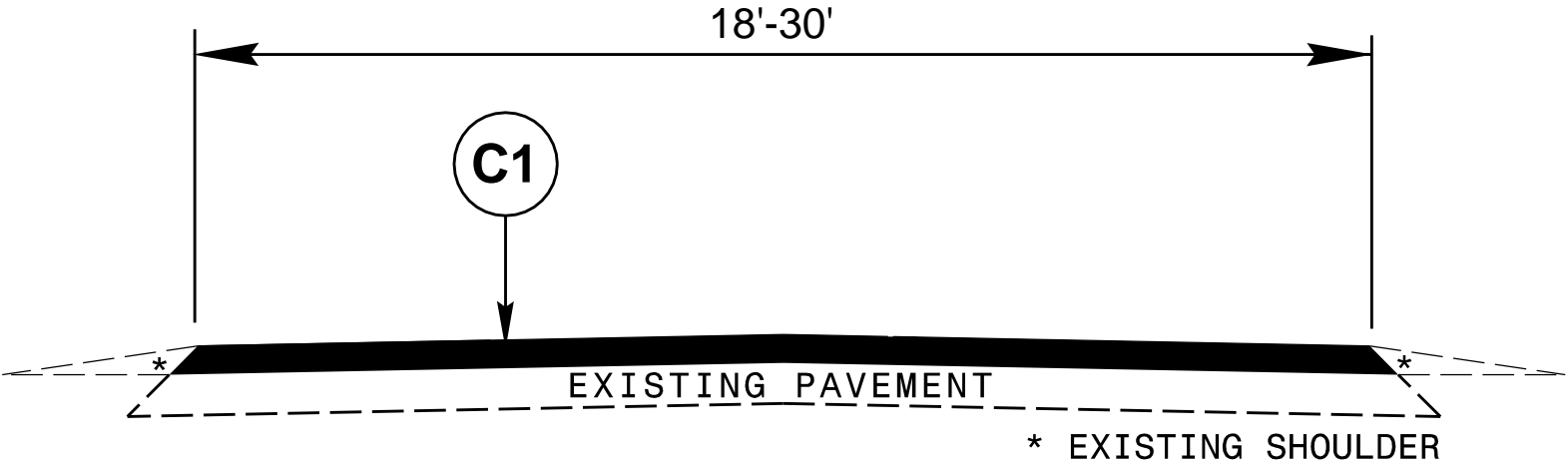


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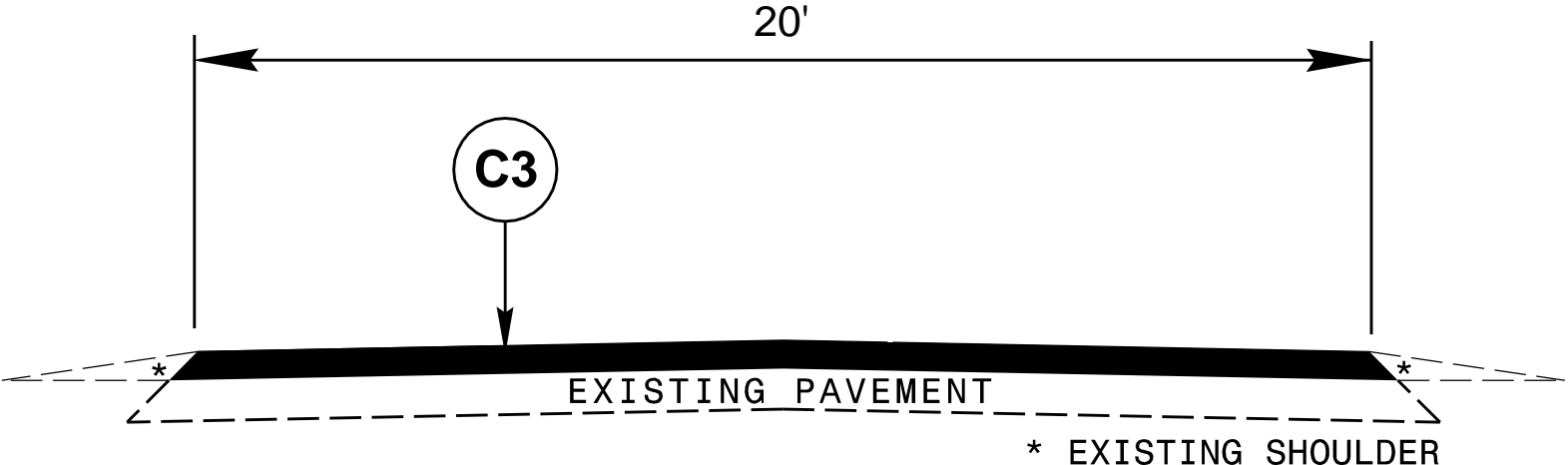


TYPICAL SECTION #3

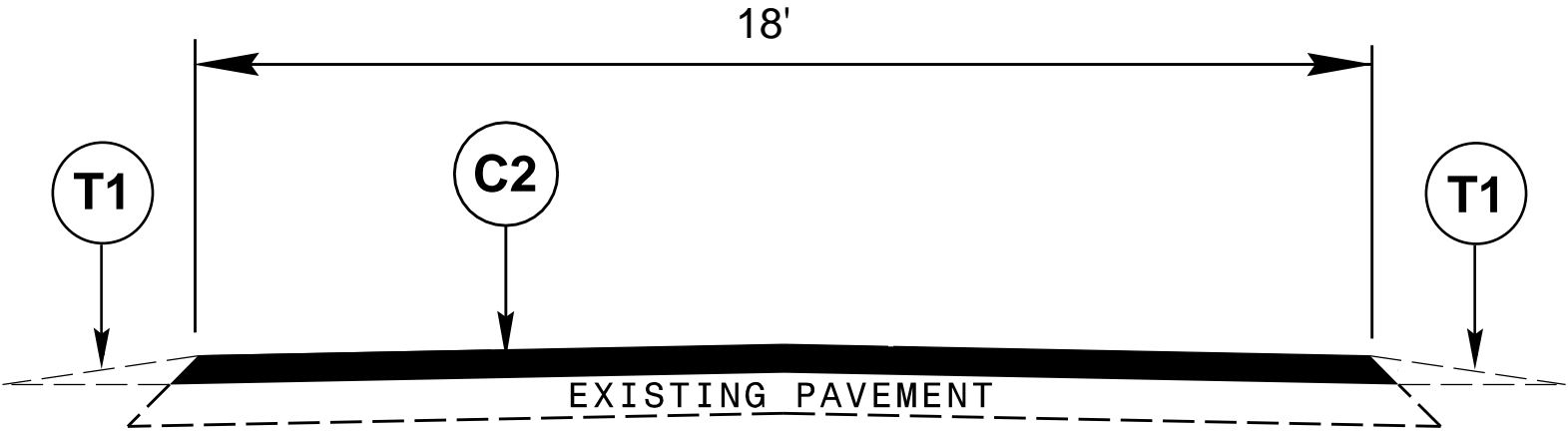
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YARD
C2	PROP. APPROX. 1.25" ASPHALTIC CONCRETE SURFACE COURSE, TYPE 9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YARD
C3	PATCHING
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F3	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT 0" TO 1" DEPTH
V2	INCIDENTAL MILLING



TYPICAL SECTION #4

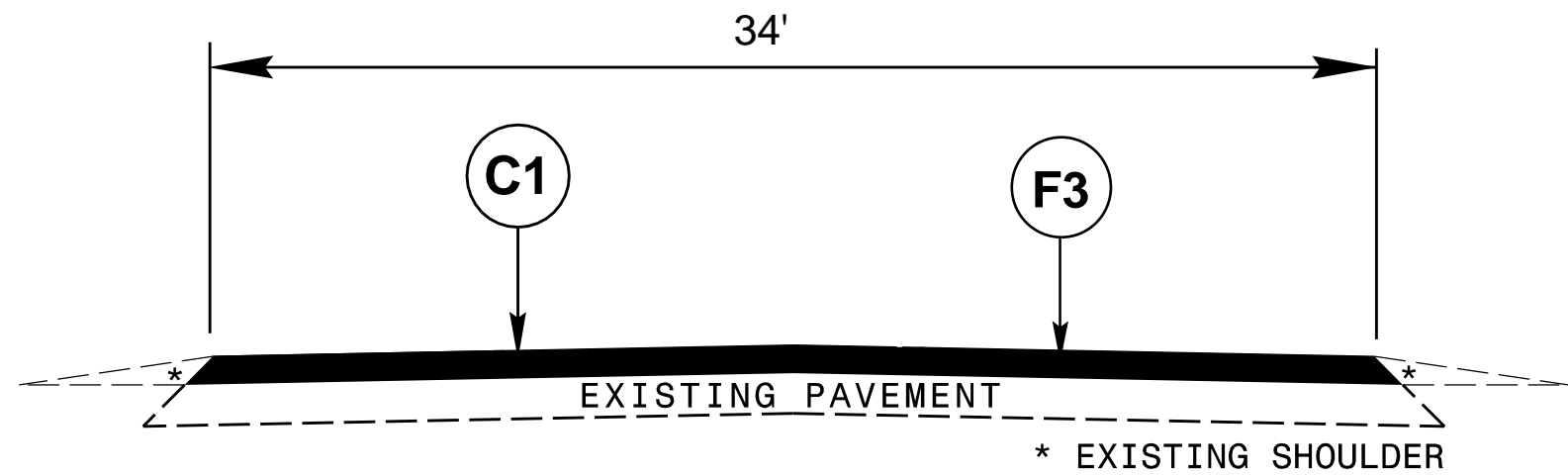


TYPICAL SECTION #5



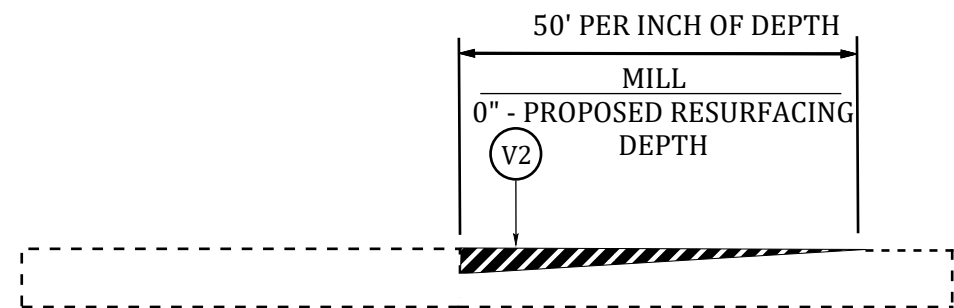
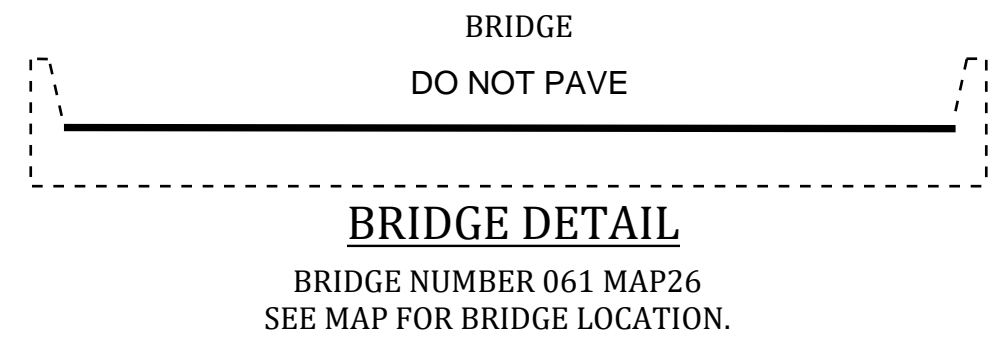
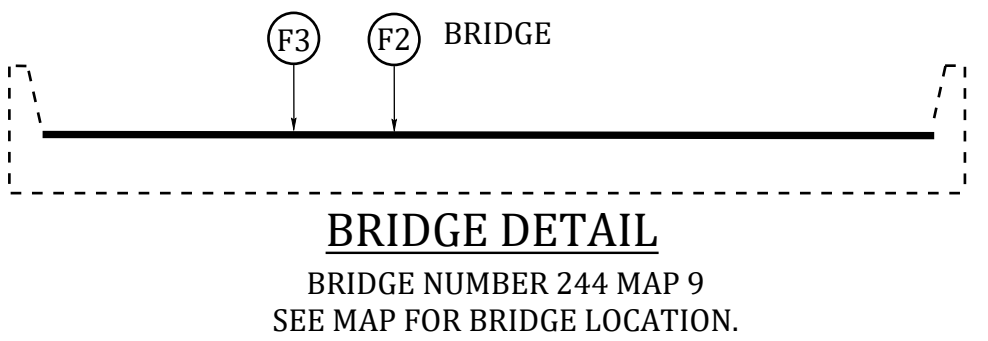
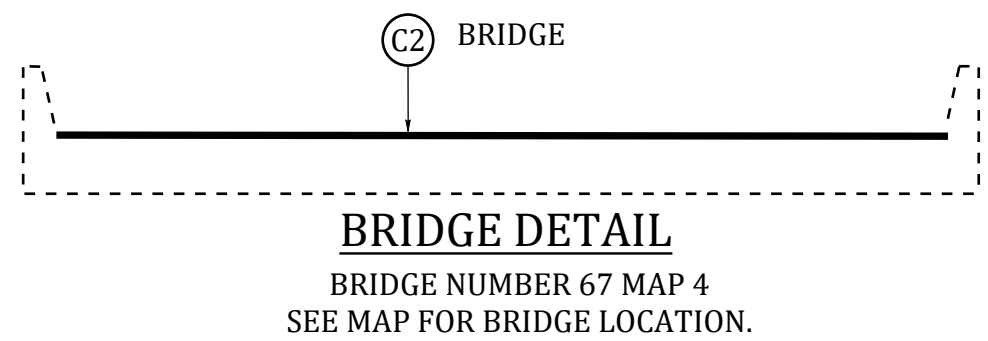
TYPICAL SECTION #6

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YARD
C2	PROP. APPROX. 1.25" ASPHALTIC CONCRETE SURFACE COURSE, TYPE 9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YARD
C3	PATCHING
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F3	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT 0" TO 1" DEPTH
V2	INCIDENTAL MILLING



TYPICAL SECTION #7

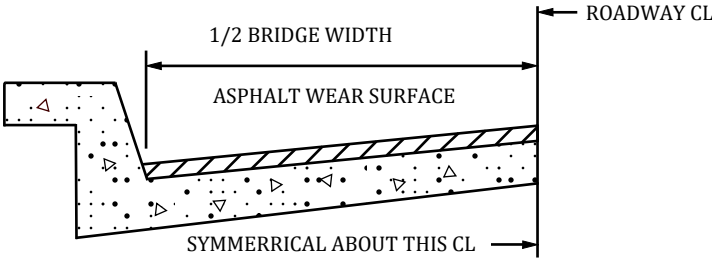
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YARD
C2	PROP. APPROX. 1.25" ASPHALTIC CONCRETE SURFACE COURSE, TYPE 9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YARD
C3	PATCHING
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F3	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT 0" TO 1" DEPTH
V2	INCIDENTAL MILLING



MILLING DETAIL TO TIE INTO EXIST PAVEMENT

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT HE WILL BE REQUIRED TO MILL THE EXISTING ASPHALT PAVEMENT TO ENSURE A PROPER TIE-IN WITH THE EXISTING SURFACE AT THE BEGINNING, END AND Y LINES OF EACH MAP TO BE RESURFACED WITH ASPHALT CONC SURFACE COURSE, TYPE 9.5B OR S9.5C. THIS WILL BE PAID FOR AS INCIDENTAL MILLING.

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YARD
C2	PROP. APPROX. 1.25" ASPHALTIC CONCRETE SURFACE COURSE, TYPE 9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YARD
C3	PATCHING
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F3	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT 0" TO 1" DEPTH
V2	INCIDENTAL MILLING



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN.

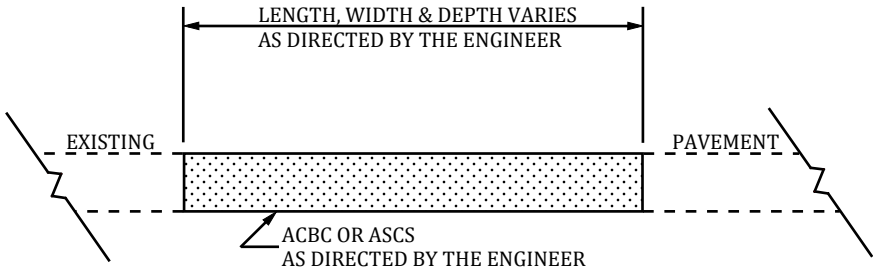
THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1/2", S9.5B 1", S9.5C,D 1.5" - 2". ULTRA-THIN HOT MIX ASPHALT ".

NOTES

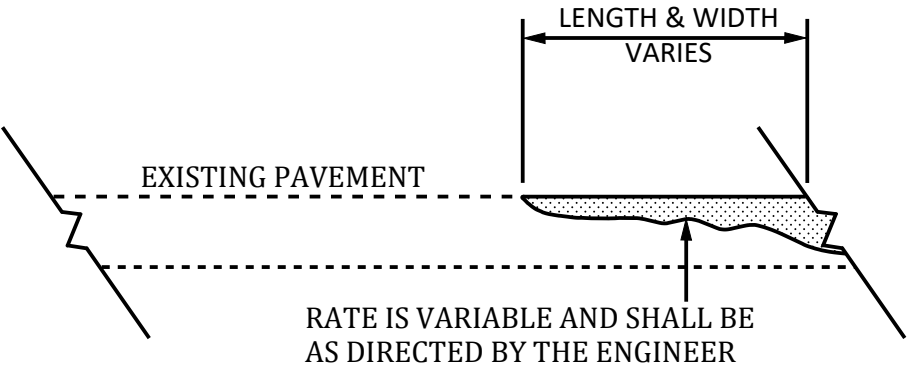
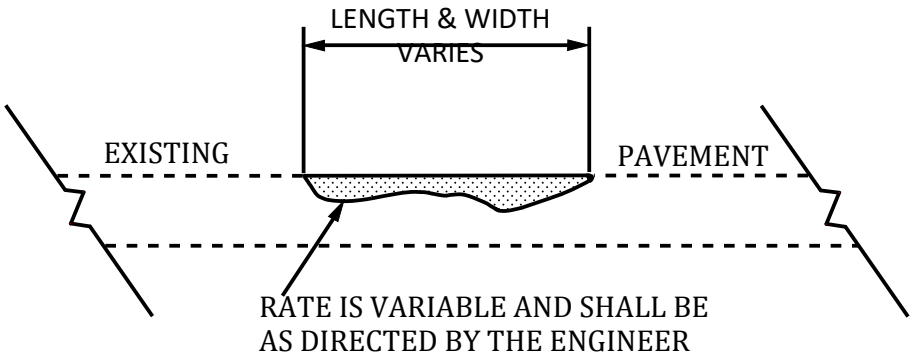
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.

SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.

BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

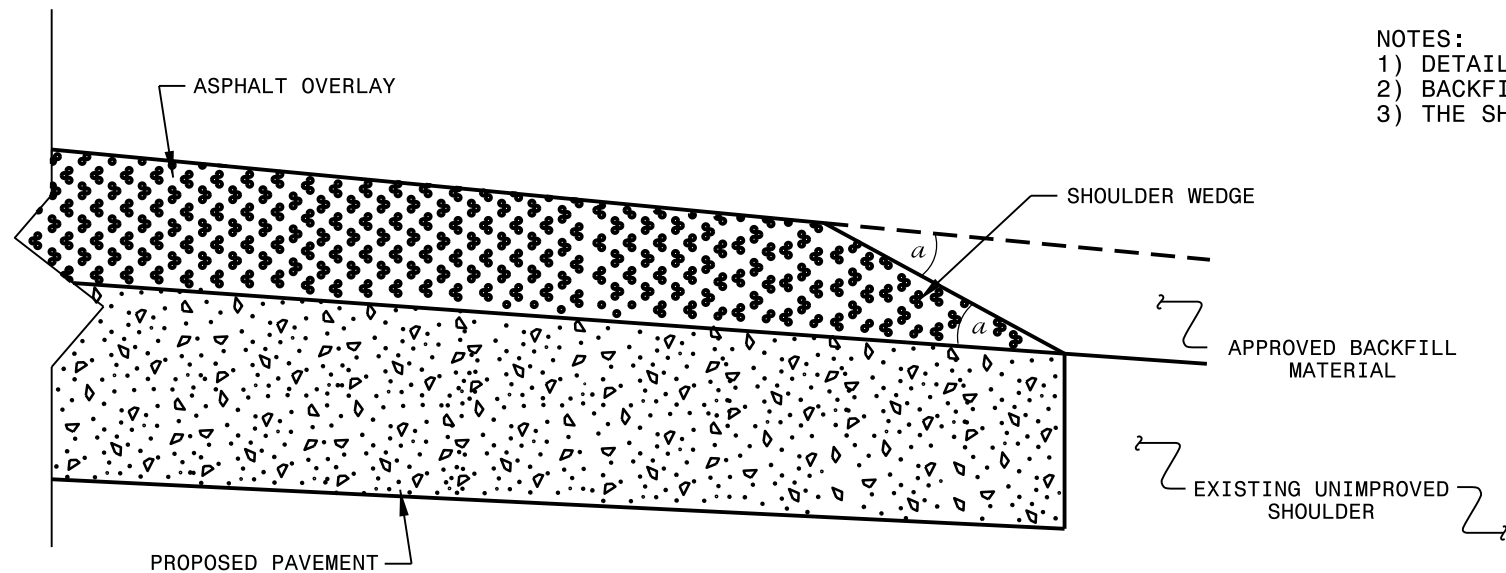


PATCHING EXISTING PAVEMENT

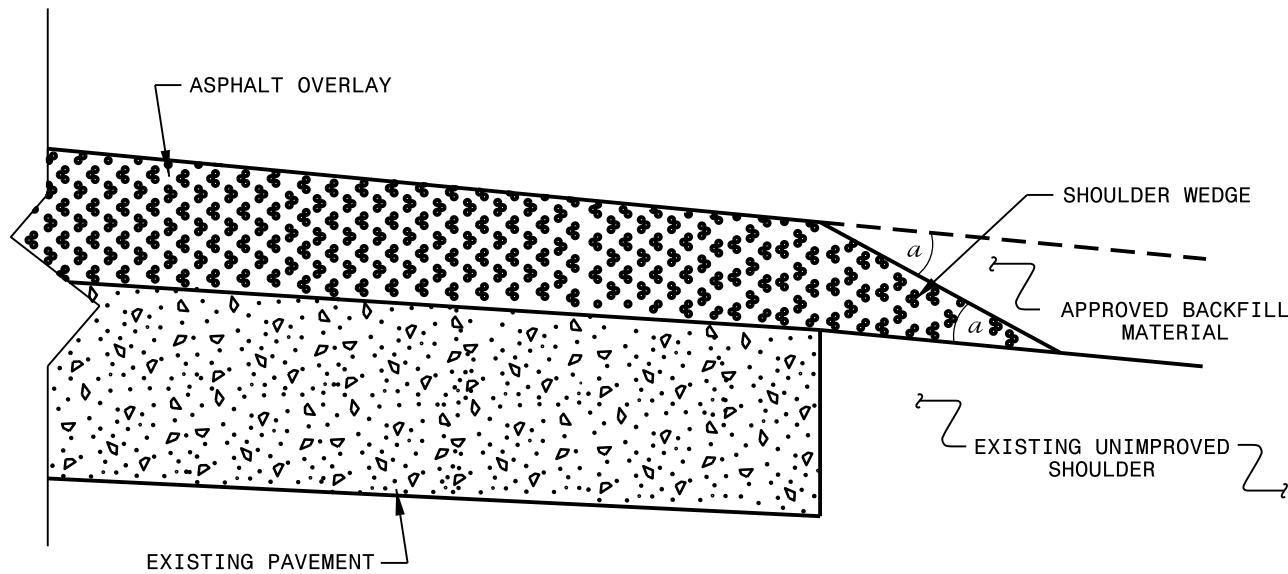


DETAIL SHOWING METHOD OF WEDGING

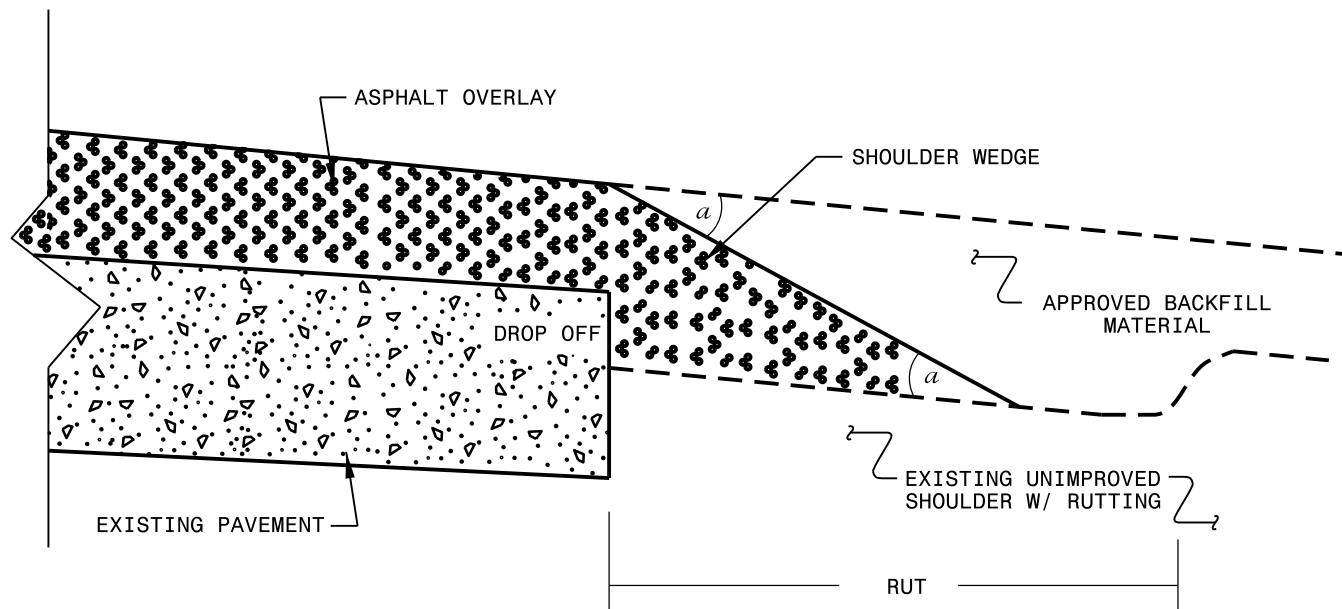
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or
with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)

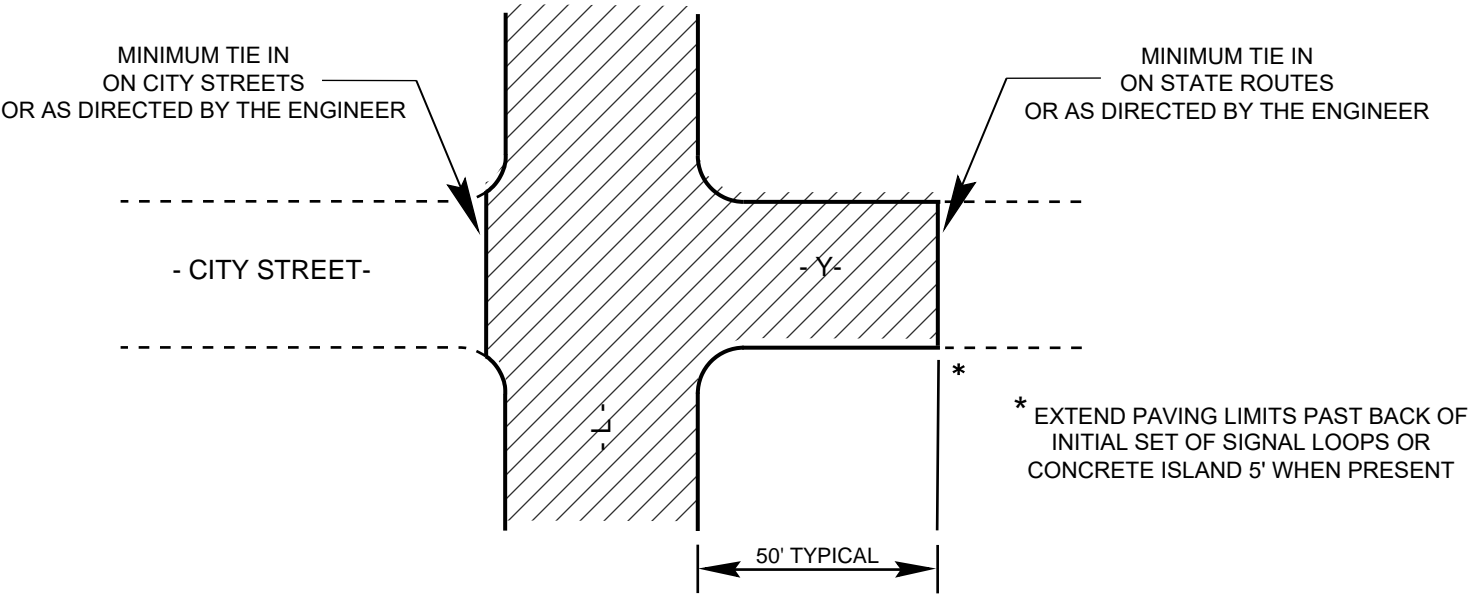


SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to
Rutted Shoulder)

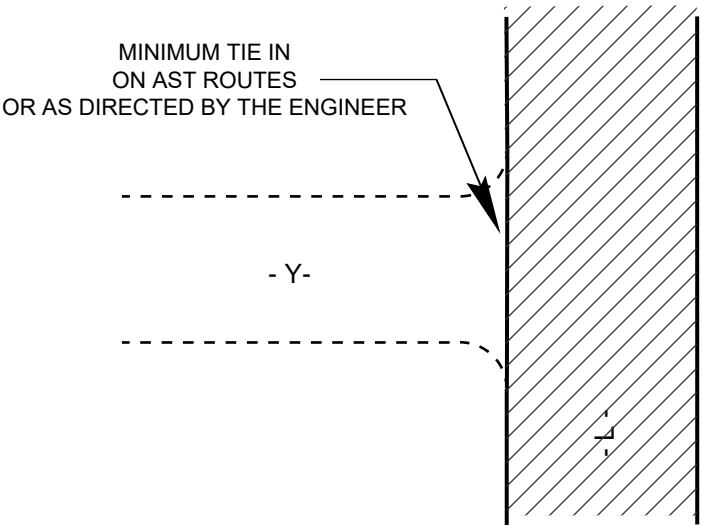
- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY:	T. SPELL	DATE:	7-19-11
MODIFIED BY:		DATE:	10/16/12
CHECKED BY:		DATE:	
FILE SPEC.:	s:\usr\details\stand\shoulderwedgedetail.dgn		

DETAIL 1



DETAIL 2



TYPICAL DETAILS OF PAVING LIMITS AT -Y- LINES

BRIDGE DATA

[illegible]

SUMMARY OF QUANTITIES

MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE					1220000000-E	1245000000-E	1260000000-E	1308000000-E	1330000000-E	1519000000-E	1526000000-E	1575000000-E	1704000000-E	1803500000-E	1814500000-E	1820000000-E	1838000000-E
						LENGTH	WIDTH	BEGI N MP	END MP	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	AGGREGATE SHOULDER BORROW	MILLING ASPHALT PAVEMENT, 0"TO 1"	INCIDENTAL MILLING	ASPHALT CONC SURFACE COURSE, TYPE S9.5B	ASPHALT CONC SURFACE COURSE, TYPE S4.75A	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, DOUBLE SEAL	ASPHALT SURFACE TREATMENT, SINGLE SEAL	ASPHALT SURFACE TREATMENT, FOG SEAL	EMULSION FOR ASPHALT SURFACE TREATMENT
										TON	SMI	TON	SY	SY	TON	TON	TON	TON	SY	SY	SY	GAL
1	SR 1733 (CLARKS CHAPEL ROAD)	FROM SR 2207 (OLD MARS HILL HIGHWAY) TO SR 1737 (CLARKS CHAPEL EXTENSION)	1	2	2WU	1.11	20.00	0.00	1.11								16	325	13,099		13,099	7,860
2	SR 1737 (CLARKS CHAPEL EXTENSION)	FROM SR 1764 (FLAT CREEK CHURCH ROAD) TO SR 1733 (CLARKS CHAPELROAD)	1	2	2WU	0.98	20.00	0.00	0.98								5	98	11,593		11,593	6,956
3	SR 1738 (OLD GIBBS ROAD)	FROM SR 1737 (CLARKS CHAPEL EXTENSION) TO SR 1740 (NEW STOCK ROAD)	1	2	2WU	1.32	18.00	0.00	1.32										13,989		13,989	8,393
4	SR 1740 (NEW STOCK ROAD)	FROM US 25/70 PAVEMENT CHANGE TO SR 1764 (PANTER BRRANCH ROAD) PAVEMENT CHANGE	2	2	2WU	1.74	21.00	1.51	3.25	32	3.48	453		235	1,590		116	257		21,437		6,431
5	SR 1746 (HUGHEY ROAD)	FROM MADISON COUNTY LINE TO SR 1745 (PANTER BRANCH ROAD)	1	2	2WU	1.16	19.00	0.00	1.16								2	50	12,930		12,930	7,758
6	SR 1749 (GRANDVIEW ROAD)	FROM SR 1743 (FLINT HILL ROAD) TO MADISON COUNTY LINE	1	2	2WU	0.26	18.00	0.00	0.26								4	80	2,846		2,846	1,708
7	SR 1755 (INDIAN CAMP BRANCH ROAD)	FROM SR 1741 (SHEPPARD BRANCH ROAD) TO SR 1756 (JUPITER ROAD)	1	2	2WU	1.38	18.00	0.00	1.38								4	80	14,773		14,773	8,664
8	SR 1759 (PICKENS ROAD)	FROM SR 1757 (PALMER FORD ROAD) TO SR 1761 (LOCUST GROVE ROAD)	1	2	2WU	1.16	19.00	0.00	1.16								8	170	13,130		13,130	7,878
9	SR 1761 (LOCUST GROVE ROAD)	FROM SR 1756 (JUPITER ROAD) TO SR 1768 (OLD BURNSVILLE ROAD)	1	2	2WU	2.27	19.00	0.00	2.27								13	265	25,503		25,503	15,302
10	SR 1763 (MCLEAN ROAD)	FROM SR 1756 (JUPITER ROAD) TO SR 2207 (OLD MARS HILL HIGHWAY)	1	2	2WU	0.95	19.00	0.00	0.95								5	100	10,648		10,648	6,388
11	SR 1767 (MCLEAN ROAD)	FROM SR 1763 (BELL SPRINKLR ROAD) TO SR 2207 (OLD MARS HILL ROAD)	1	2	2WU	0.62	19.00	0.00	0.62								3	65	6,960		6,960	4,176
12	SR 1768 (LOCUST GROVE ROAD/OLD BURNSVILLE ROAD)	FROM SR 1768 (ALLMAN HILL ROAD) TO SR 1768 (OLD BURNSVILLE ROAD)	1	2	2WU	0.12	18.00	1.30	1.42								1	30	1,600		1,600	960
13	SR 1770 (LOCUST GROVE ROAD)	FROM SR 2207 (OLD MARS HILL HIGHWAY) TO SR 1768 (ALLMAN HILL ROAD)	1	2	2WU	0.75	18.00	0.00	0.75								8	165	8,005		8,005	4,803
14	SR 1786 (QUAIL COVE ROAD)	FROM SR 1882 (WOODLAND HILLS ROAD) TO CUL DE SAC	3	2	2WU	0.04	28.00	0.00	0.04				250	312		50	4	11				
15	SR 1787 (SQUIRREL HILL ROAD)	FROM SR 1882 (WOODLAND HILLS ROAD) TO CUL DE SAC	3	2	2WU	0.03	26.00	0.01	0.04				276	289		32	3	11				
16	SR 1826 (CHADWICK WADE DRIVE)	FROM SR 1883 (AKIEN ROAD) TO END OF MAINTENANCE	4	2	2WU	0.27	20.50	0.01	0.28					114		210	16	20				
17	SR 1831 (WILDFLOWER ROAD)	FROM SR 1830 (ACORN ROAD) TO CUL DE SAC	3	2	2WU	0.12	29.00	0.00	0.12				1,127	323		135	11	30				
18	SR 1834 (GARRISON BRANCH NO2 ROAD)	FROM US 25/70 PAVEMENT JOINT TO SR 1874 (HORIZON HILLS DRIVE)	4	2	2WU	0.16	26.00	0.02	0.18					289		80	8	40				
19	SR 1835 (GARRISON BRANCH NO3 ROAD)	FROM US/25/70 PAVEMENT JOINT TO END OF MAINTENANCE	4	2	2WU	0.21	26.00	0.03	0.24					289		210	20	100				
20	SR 1844 (WOODY LANE)	FROM BLUEBIRD RIDGE (CITY STREET) AT PAVEMENT JOINT TO END OF MAINTENANCE / CHURCH PARKING	4	2	2WU	0.28	30.00	0.00	0.28					334		237	18	25				
21	SR 1847 (LACKEY LANE)	FROM SR 1720 (AIKEN ROAD) TO CUL DE SAC	4	2	2WU	0.37	21.00	0.01	0.38					117		275	22	60				
22	SR 1857 (HIGGINS ROAD)	FROM SR 1738 (OLD GIBBS ROAD) TO END OF MAINTENANCE	1	2	2WU	0.32	19.00	0.00	0.32								3	55	3,642		3,642	2,186
23	SR 1872 (SAWMILL ROAD)	FROM SR 1741 (SHEPPARD BRANCH ROAD) TO END OF MAINTENANCE	1	2	2WU	0.17	18.00	0.00	0.17								1	20	1,820		1,820	1,092
24	SR 1874 (HORIZON HILLS DRIVE)	FROM SR 1834 (GARRISON BRANCH #2 ROAD) TO SR 1875 (SCENIC VIEW DRIVE)	4	2	2WU	0.12	20.00	0.00	0.12							80	7	25				
25	SR 1879 (HAMPTON LANE)	FROM SR 1764 (FLAT CREEK CHURCH ROAD) TO END OF MAINTENANCE	4	2	2WU	0.29	18.00	0.01	0.30					200		175	15	60				
26	SR 1882 (NEW STOCK ROAD)	FROM SR 1883 (AIKEN ROAD) - .14 MI. TO SR 1727 (MONTICELLO ROAD)	5	2	2WU	1.76	20.00	0.70	2.46								13	280				
27	SR 1895 (AUTUMN HILLS DRIVE)	FROM SR 1835 (GARRISON BRANCH #3 ROAD) TO CUL DE SAC	4	2	2WU	0.59	19.00	0.00	0.59					212		387	32	95				
28	SR 1896 (AMBER KNOLL COURT)	FROM SR 1895 (AUTUMN HILLS DRIVE) TO CUL DE SAC	4	2	2WU	0.09	20.00	0.00	0.09					223		75	6	15				
29	SR 1897 (SCARLET RIDGE)	FROM SR 1895 (AUTUMN HILLS DRIVE) TO CUL DE SAC	4	2	2WU	0.11	20.50	0.00	0.11					228		95	8	20				
30	SR 1900 (JUPITER HILLS DRIVE)	FROM SR 1756 (JUPITER ROAD) TO SR 1756 (JUPITER ROAD)	4	2	2WU	0.47	19.00	0.01	0.48					212		290	23	65				
PAGE TOTAL FOR PROJ NO. 2025CPT.13.08.20112						19.22				32	3.48	453	1,653	3,377	1,590	2,331	395	2,617	140,538	21,437	140,538	90,555

NOTE ALL MAPS, PROJECT NUMBER = 2025CPT.13.08.20112, COUNTY = BUNCOMBE, WARM MIX REQUIRED = NO, FINAL SURFACE TESTING REQUIRED = NO.

SUMMARY OF QUANTITIES

										1220000000-E	1245000000-E	1260000000-E	1308000000-E	1330000000-E	1519000000-E	1526000000-E	1575000000-E	1704000000-E	1803500000-E	1814500000-E	1820000000-E	1838000000-E
MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGI N	END MP	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	AGGREGATE SHOULDER BORROW	MILLING ASPHALT PAVEMENT, 0"TO 1"	INCIDENTAL MILLING	ASPHALT CONC SURFACE COURSE, TYPE S9.5B	ASPHALT CONC SURFACE COURSE, TYPE S4.75A	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, DOUBLE SEAL	ASPHALT SURFACE TREATMENT, SINGLE SEAL	ASPHALT SURFACE TREATMENT, FOG SEAL	EMULSION FOR ASPHALT SURFACE TREATMENT
						MI	FT			TON	SMI	TON	SY	SY	TON	TON	TON	TON	SY	SY	SY	GAL
31	SR-2096 (GARRISON ROAD)	FROM US 19 TO SR 2094 (STONEY KNOB ROAD)	6	2	2WU	0.23	18.00	0.01	0.24	16	0.46	60		100	185		18	125				
32	SR 2122 (UNION CHAPEL ROAD)	FROM SR 1003 (REEMS CREEK ROAD) TO END OF PAVEMENT	1	2	2WU	1.13	19.00	0.01	1.14								4	90	12,596		12,596	7,558
33	SR 2127 (SOUTH MAIN STREET)	FROM SR 1003 (REEMS CREEK ROAD) TO US 19 (MERRIMON AVENUE)	7	2	2WU	0.72	34.00	0.00	0.72								4	90			11,784	
TOTAL FOR PROJ NO. 2025CPT.13.08.20112						2.08				16	0.46	60		100	185		26	305	12,596		24,380	7,558
GRAND TOTAL						21.30				48	4	513	1,653	3,477	1,775	2,331	421	2,922	153,134	21,437	164,918	98,113

NOTE ALL MAPS, PROJECT NUMBER = 2025CPT.13.08.20112, COUNTY = BUNCOMBE, WARM MIX REQUIRED = NO, FINAL SURFACE TESTING REQUIRED = NO.

PROJECT NO.	SHEET NO.
2025CPT.13.08.20112	16

THERMOPLASTIC AND PAINT QUANTITIES

MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	4413000000-E WORK ZONE ADVANCE / GENERAL WARNING SIGNING	4457000000-N TEMPORARY TRAFFIC CONTROL	4695000000-E THERMOPLASTIC PAVEMENT MARKING LINES (8", 90 MILS) WHITE	4709000000-E THERMOPLASTIC PAVEMENT MARKING LINES (24", 90 MILS) WHITE STOP BAR	4709000000-E THERMOPLASTIC PAVEMENT MARKING LINES (24", 90 MILS) WHITE HI- VISIBILTY CROSSWALK	4720000000-E THERMOPLASTIC PAVEMENT MARKING CHARACTER (90 MILS) SCHOOL	4890000000-E HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINES (4", 65 MILS) WHITE	4890000000-E HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINES (4", 65 MILS) YELLOW
						MI	FT			SF	LS	LF	LF	LF	EA	LF	LF
1	SR 1733 (CLARKS CHAPEL ROAD)	FROM SR 2207 (OLD MARS HILL HIGHWAY) TO SR 1737 (CLARKS CHAPEL EXTENSION)	1	2	2WU	1.11	20.00	0.00	1.11	124	*		80			11,722	11,722
2	SR 1737 (CLARKS CHAPEL EXTENSION)	FROM SR 1764 (FLAT CREEK CHURCH ROAD) TO SR 1733 (CLARKS CHAPELROAD)	1	2	2WU	0.98	20.00	0.00	0.98	110						10,350	10,350
3	SR 1738 (OLD GIBBS ROAD)	FROM SR 1737 (CLARKS CHAPEL EXTENSION) TO SR 1740 (NEW STOCK ROAD)	1	2	2WU	1.32	18.00	0.00	1.32	148						13,833	13,833
4	SR 1740 (NEW STOCK ROAD)	FROM US 25/70 PAVEMENT CHANGE TO SR 1764 (PANTER BRRANCH ROAD) PAVEMENT CHANGE	2	2	2WU	1.74	21.00	1.51	3.25	195						18,374	18,374
5	SR 1746 (HUGHEY ROAD)	FROM MADISON COUNTY LINE TO SR 1745 (PANTER BRANCH ROAD)	1	2	2WU	1.16	19.00	0.00	1.16	130						12,250	12,250
6	SR 1749 (GRANDVIEW ROAD)	FROM SR 1743 (FLINT HILL ROAD) TO MADISON COUNTY LINE	1	2	2WU	0.26	18.00	0.00	0.26	30						2,746	2,746
7	SR 1755 (INDIAN CAMP BRANCH ROAD)	FROM SR 1741 (SHEPPARD BRANCH ROAD) TO SR 1756 (JUPITER ROAD)	1	2	2WU	1.38	18.00	0.00	1.38	155						14,468	14,468
8	SR 1759 (PICKENS ROAD)	FROM SR 1757 (PALMER FORD ROAD) TO SR 1761 (LOCUST GROVE ROAD)	1	2	2WU	1.16	19.00	0.00	1.16	130						12,250	12,250
9	SR 1761 (LOCUST GROVE ROAD)	FROM SR 1756 (JUPITER ROAD) TO SR 1768 (OLD BURNSVILLE ROAD)	1	2	2WU	2.27	19.00	0.00	2.27	155		76				23,971	23,971
10	SR 1763 (MCLEAN ROAD)	FROM SR 1756 (JUPITER ROAD) TO SR 2207 (OLD MARS HILL HIGHWAY)	1	2	2WU	0.95	19.00	0.00	0.95	104						10,032	10,032
11	SR 1767 (MCLEAN ROAD)	FROM SR 1763 (BELL SPRINKLR ROAD) TO SR 2207 (OLD MARS HILL ROAD)	1	2	2WU	0.62	19.00	0.00	0.62	70						6,548	6,548
12	SR 1768 (LOCUST GROVE ROAD/OLD BURNSVILLE ROAD)	FROM SR 1768 (ALLMAN HILL ROAD) TO SR 1768 (OLD BURNSVILLE ROAD)	1	2	2WU	0.12	18.00	1.30	1.42	14						1,267	1,267
13	SR 1770 (LOCUST GROVE ROAD)	FROM SR 2207 (OLD MARS HILL HIGHWAY) TO SR 1768 (ALLMAN HILL ROAD)	1	2	2WU	0.75	18.00	0.00	0.75	84						7,920	7,920
14	SR 1786 (QUAIL COVE ROAD)	FROM SR 1882 (WOODLAND HILLS ROAD) TO CUL DE SAC	3	2	2WU	0.04	28.00	0.00	0.04	5							
15	SR 1787 (SQUIRREL HILL ROAD)	FROM SR 1882 (WOODLAND HILLS ROAD) TO CUL DE SAC	3	2	2WU	0.03	26.00	0.01	0.04	4							
16	SR 1826 (CHADWICK WADE DRIVE)	FROM SR 1883 (AKIEN ROAD) TO END OF MAINTENANCE	4	2	2WU	0.27	20.50	0.01	0.28	32							
17	SR 1831 (WILDFLOWER ROAD)	FROM SR 1830 (ACORN ROAD) TO CUL DE SAC	3	2	2WU	0.12	29.00	0.00	0.12	14							
18	SR 1834 (GARRISON BRANCH NO2 ROAD)	FROM US 25/70 PAVEMENT JOINT TO SR 1874 (HORIZON HILLS DRIVE)	4	2	2WU	0.16	26.00	0.02	0.18	18							
19	SR 1835 (GARRISON BRANCH ROAD)	FROM US/25/70 PAVEMENT JOINT TO END OF MAINTENANCE	4	2	2WU	0.21	26.00	0.03	0.24	24							
20	SR 1844 (WOODY LANE)	FROM BLUEBIRD RIDGE (CITY STREET) AT PAVEMENT JOINT TO END OF MAINTENANCE / CHURCH PARKING	4	2	2WU	0.28	30.00	0.00	0.28	38							
21	SR 1847 (LACKEY LANE)	FROM SR 1720 (AIKEN ROAD) TO CUL DE SAC	4	2	2WU	0.37	21.00	0.01	0.38	42							
22	SR 1857 (HIGGINS ROAD)	FROM SR 1738 (OLD GIBBS ROAD) TO END OF MAINTENANCE	1	2	2WU	0.32	19.00	0.00	0.32	36							
23	SR 1872 (SAWMILL ROAD)	FROM SR 1741 (SHEPPARD BRANCH ROAD) TO END OF MAINTENANCE	1	2	2WU	0.17	18.00	0.00	0.17	19							
24	SR 1874 (HORIZON HILLS DRIVE)	FROM SR 1834 (GARRISON BRANCH #2 ROAD) TO SR 1875 (SCENIC VIEW DRIVE)	4	2	2WU	0.12	20.00	0.00	0.12	14							
25	SR 1879 (HAMPTON LANE)	FROM SR 1764 (FLAT CREEK CHURCH ROAD) TO END OF MAINTENANCE	4	2	2WU	0.29	18.00	0.01	0.30	33							
26	SR 1882 (NEW STOCK ROAD)	FROM SR 1883 (AIKEN ROAD) - .14 MI. TO SR 1727 (MONTICELLO ROAD)	5	2	2WU	1.76	20.00	0.70	2.46	197			25			18,588	18,588
27	SR 1895 (AUTUMN HILLS DRIVE)	FROM SR 1835 (GARRISON BRANCH #3 ROAD) TO CUL DE SAC	4	2	2WU	0.59	19.00	0.00	0.59	66							
28	SR 1896 (AMBER KNOLL COURT)	FROM SR 1895 (AUTUMN HILLS DRIVE) TO CUL DE SAC	4	2	2WU	0.09	20.00	0.00	0.09	10							
29	SR 1897 (SCARLET RIDGE)	FROM SR 1895 (AUTUMN HILLS DRIVE) TO CUL DE SAC	4	2	2WU	0.11	20.50	0.00	0.11	13							
30	SR 1900 (JUPITER HILLS DRIVE)	FROM SR 1756 (JUPITER ROAD) TO SR 1756 (JUPITER ROAD)	4	2	2WU	0.47	19.00	0.01	0.48	53							
PAGE TOTAL FOR PROJ NO. 2025CPT.13.08.20112						19.22				2,067	1	76	105			164,319	164,319

NOTE ALL MAPS, PROJECT NUMBER = 2025CPT.13.08.20112, COUNTY = BUNCOMBE, WARM MIX REQUIRED = NO, FINAL SURFACE TESTING REQUIRED = NO.

THERMOPLASTIC AND PAINT QUANTITIES

										4413000000-E	4457000000-N	4695000000-E	4709000000-E		4720000000-E	4890000000-E	
MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	WORK ZONE ADVANCE / GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	THERMOPLASTIC PAVEMENT MARKING LINES (8", 90 MILS) WHITE	THERMOPLASTIC PAVEMENT MARKING LINES (24", 90 MILS) WHITE STOP BAR	THERMOPLASTIC PAVEMENT MARKING LINES (24", 90 MILS) WHITE HI- VISIBILTY CROSSWALK	THERMOPLASTIC PAVEMENT MARKING CHARACTER (90 MILS) SCHOOL	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINES (4", 65 MILS) WHITE	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINES (4", 65 MILS) YELLOW
					MI	FT			SF								
31	SR-2096 (GARRISON ROAD)	FROM US 19 TO SR 2094 (STONEY KNOB ROAD)	6	2	2WU	0.23	18.00	0.01	0.24	27	*					2,535	2,535
32	SR 2122 (UNION CHAPEL ROAD)	FROM SR 1003 (REEMS CREEK ROAD) TO END OF PAVEMENT	1	2	2WU	1.13	19.00	0.01	1.14	127						12,039	12,039
33	SR 2127 (SOUTH MAIN STREET)	FROM SR 1003 (REEMS CREEK ROAD) TO US 19 (MERRIMON AVENUE)	7	2	2WU	0.72	34.00	0.00	0.72	81			61	606	24	3,645	7,613
PAGE TOTAL FOR PROJ NO. 2025CPT.13.08.20112						2.08				235	1		61	606	24	18,219	22,187
													667			40,406	
GRAND TOTAL						21.30				2,302	1	76	166	606	24	182,538	186,506
													772			369,044	

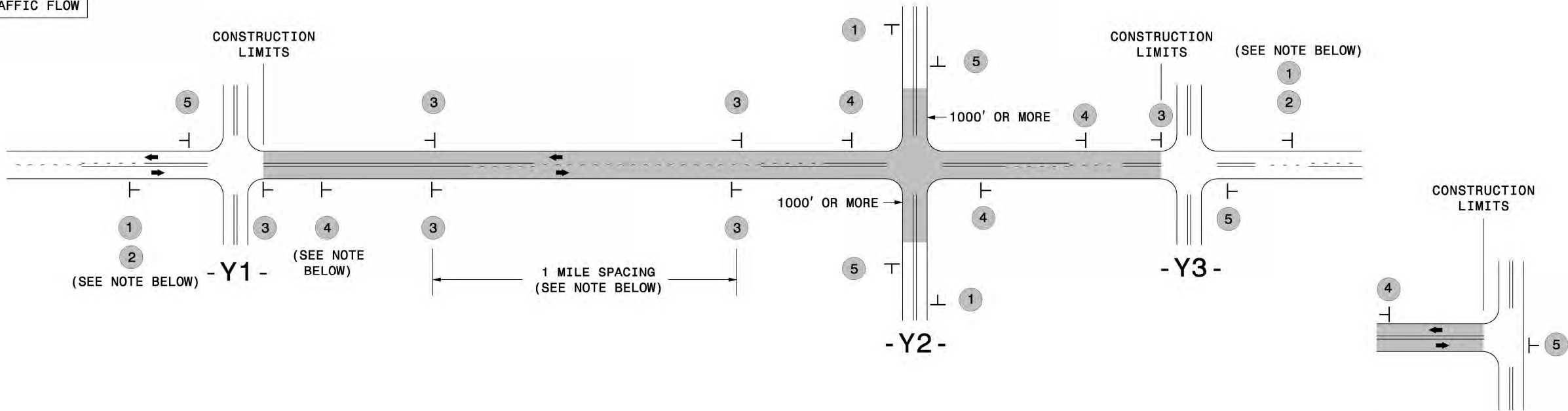
NOTE ALL MAPS, PROJECT NUMBER = 2025CPT.13.08.20112, COUNTY = BUNCOMBE, WARM MIX REQUIRED = NO, FINAL SURFACE TESTING REQUIRED = NO.

SIGNING FOR RESURFACING PROJECTS

LEGEND

STATIONARY SIGN

DIRECTION OF TRAFFIC FLOW




TEE INTERSECTION

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<div>1</div> <div>2</div> <div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div><div>NEXT XX MILES</div><div>W7-3aP 24" X 18"</div></div> <div>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</div> <div>#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER.(NO FRACTIONAL OR DECIMAL NUMBERS)</div>	NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.
	<div>3</div> <div><div>LOW/SOFT SHOULDER</div><div>SP 13107 48" X 48"</div></div> <div>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</div>	
	<div>4</div> <div><div>ROAD UNDER CONST</div><div>SP 13106 48" X 48"</div></div> <div>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</div>	
	<div>5</div> <div><div>END ROAD WORK</div><div>G20-2 A 48" X 24"</div></div> <div>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</div>	
	THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.	
LESS 2 MILES	FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.	

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE * * * * * TRAFFIC CONTROL

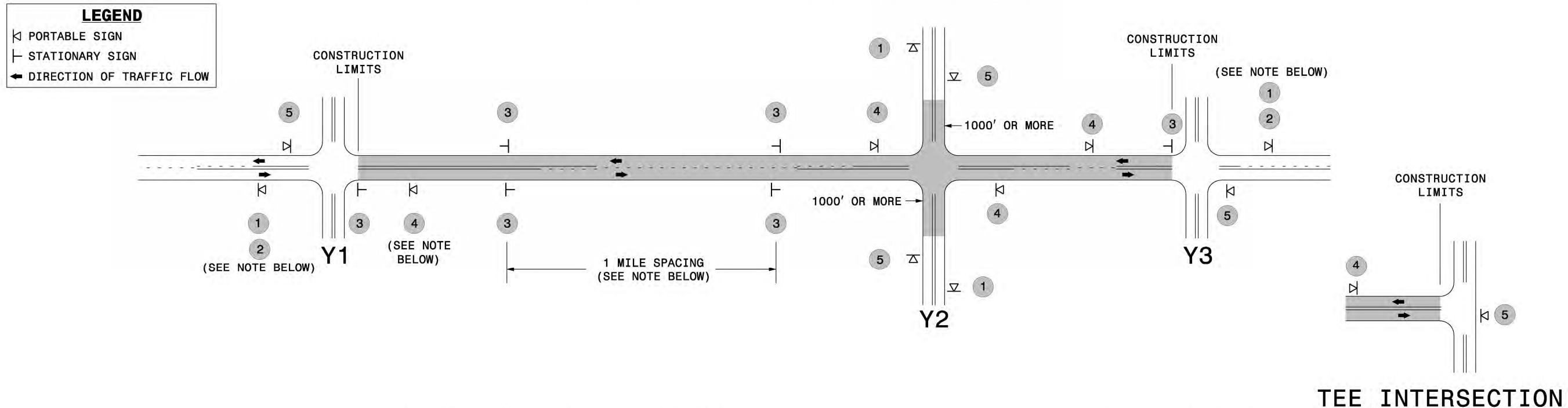


AD



ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2-LANE ROADWAY
RESURFACING

SIGNING FOR ASPHALT SURFACE TREATMENT



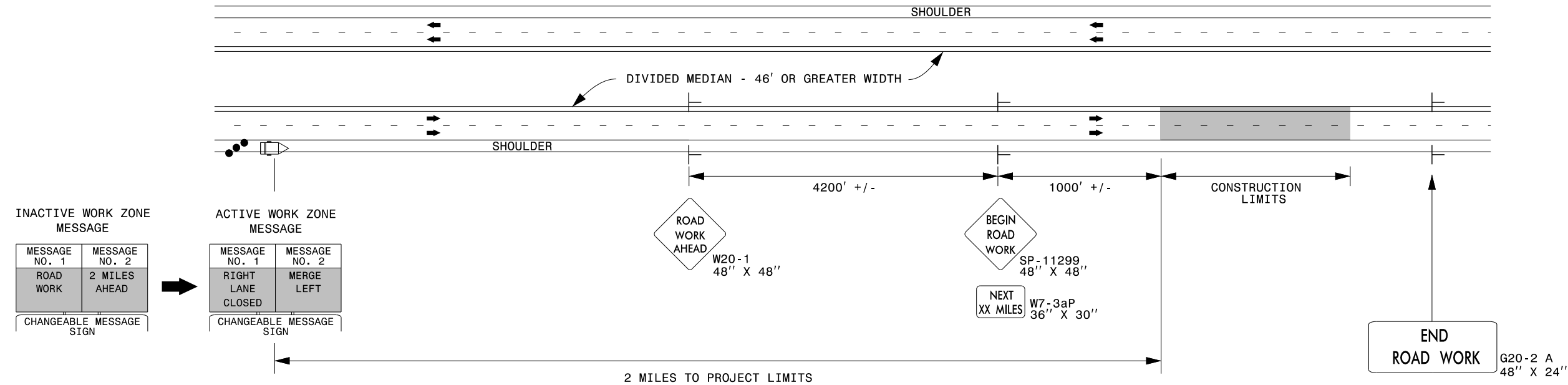
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

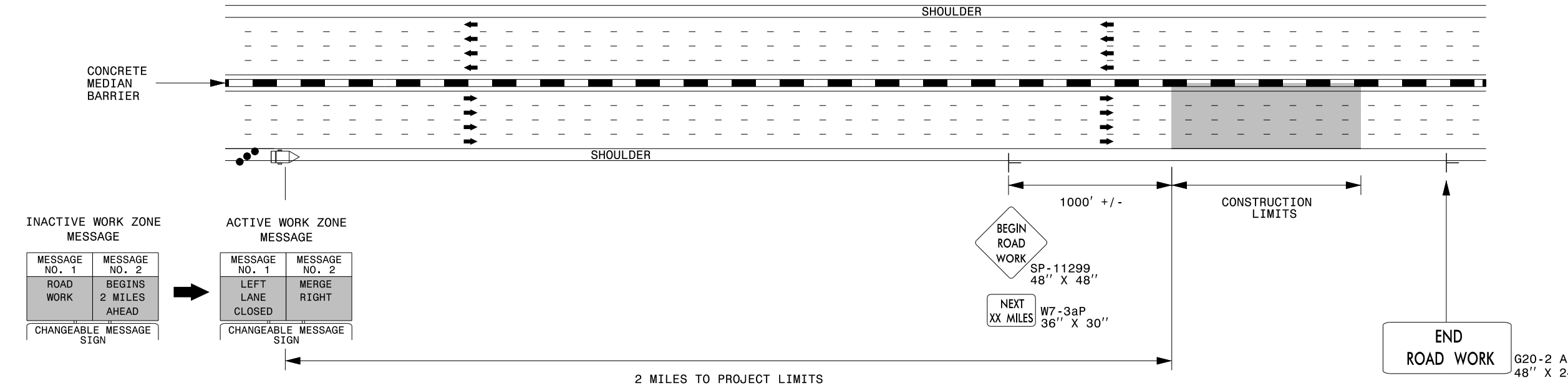
SIGNING NOTES AND PLACEMENT PER DIRECTION	<div><div>1</div><div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div></div></div> <div><div>2</div><div><div>NEXT XX MILES</div><div>W7-3aP 24" X 18"</div></div></div> <div><div>- PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</div><div><div>- SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.</div></div></div>	STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALLE BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK. <div><div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div></div><div>PLACED 500' IN ADVANCE OF FLAGGER.</div></div> <div><div><div><div>ROAD WORK AHEAD</div><div>W20-7 A 48" X 48"</div></div><div>PLACED 250' IN ADVANCE OF FLAGGER.</div></div></div>
	<div><div>3</div><div><div>LOOSE GRAVEL</div><div>W8-7 48" X 48"</div></div><div><div>UNMARKED PAVEMENT</div><div>SP 48" X 48"</div></div></div> <div><div>- ALTERNATE THE FOLLOWING TWO SIGNS:</div><div><div>- STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".</div><div><div>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER.</div><div><div>- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</div></div></div></div></div>	
	<div><div>4</div><div><div>ROAD UNDER CONST</div><div>SP 13106 48" X 48"</div></div></div> <div><div>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</div><div><div>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</div><div><div>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</div><div><div>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</div></div></div></div></div>	
	<div><div>5</div><div><div>END ROAD WORK</div><div>G20-2 A 48" X 24"</div></div></div> <div><div>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</div></div>	
	THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.	
MAPS LESS THAN 2 MILES	FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.	

<div><div><div>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</div><div><div></div></div></div></div>	ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT
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DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER



- NOTES:**
- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
 - 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
 - 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
 - 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
 - 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
 - 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

CHANGEABLE MESSAGE SIGN (CMS)

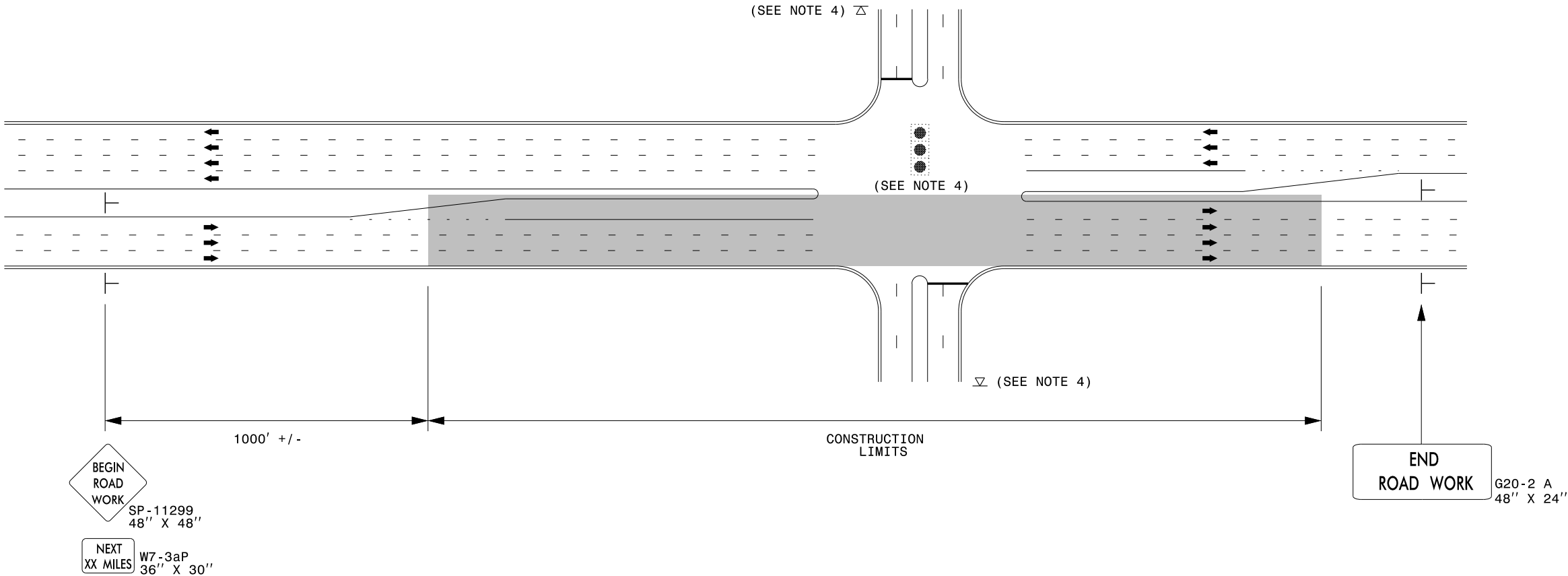
STATIONARY SIGN

DIRECTION OF TRAFFIC FLOW

TRAFFIC DRUM

RESURFACING ADVANCE WARNING SIGNS FOR HIGH SPEED FACILITIES ≥ 60 MPH

URBAN / SUBURBAN WORKZONES



NOTES:

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS.THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

STATIONARY SIGN

DIRECTION OF TRAFFIC FLOW

DIVISION OF HIGHWAYS

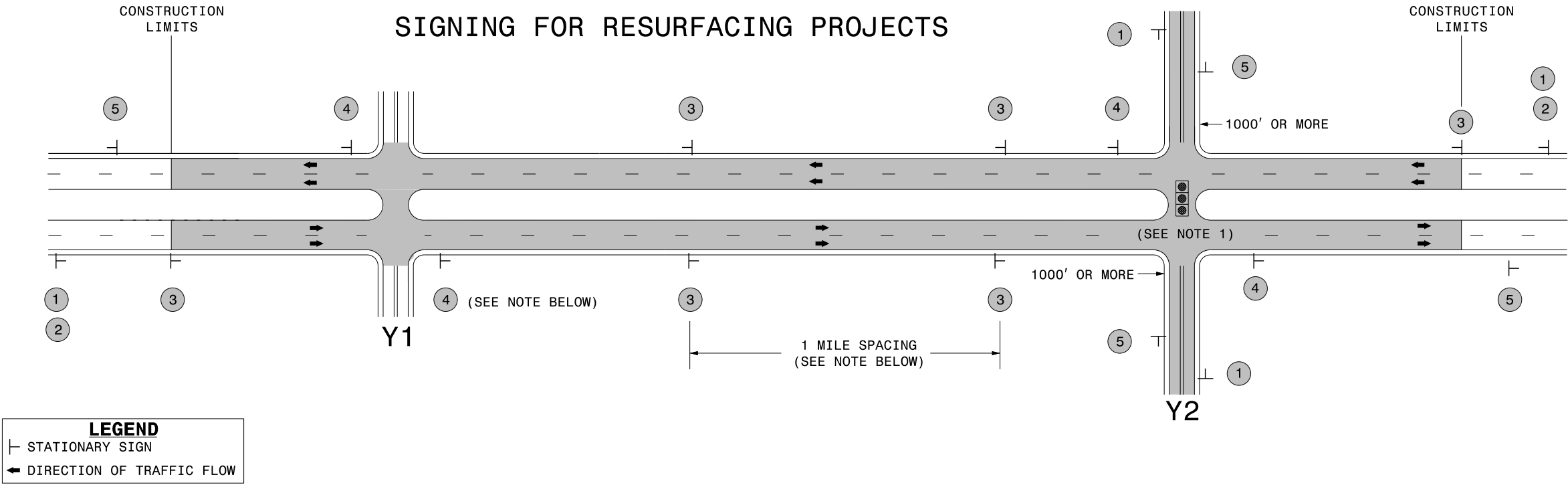
STATE OF NORTH CAROLINA

DEPARTMENT OF TRANSPORTATION

WORK ZONE TRAFFIC CONTROL

RESURFACING ADVANCE WARNING SIGNS FOR URBAN / SUBURBAN FACILITIES

3/23/2015
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User:rmgarrett



SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1		NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:	
	2		1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS	
	3		WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.	
	4			
			PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.	
	5		NOTES: 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.	